

TOWN OF NEWTOWN, CONNECTICUT

REQUEST FOR PROPOSAL

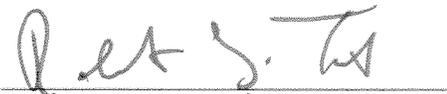
Proposals will be received at the office of the Financial Director, 3 Primrose Street, Newtown, Connecticut 06470, until but no later than: **2:00 p.m., Friday, June 15, 2012**

Covering: **Design & Build Services for the Sandy Hook Streetscape Improvement Project**  
(Mandatory Pre-bid Meeting: 9:00 am, Thursday, May 24, 2012  
Newtown Municipal Center, Council Chambers, 3 Primrose St.,  
Newtown, CT 06470)

Specs and Plans will be available on-line at [www.newtown-ct.gov](http://www.newtown-ct.gov) or at the pre-proposal meeting.

The Purchasing Authority of the Town of Newtown reserves the right to accept or reject any or all options or proposals; to waive any technicality in any proposal or part thereof, and to accept any proposal deemed to be in the best interest of the Town of Newtown. The Town of Newtown is an Affirmative Action Employer - MBE/WBE are encouraged to bid.

  
\_\_\_\_\_  
E. Patricia Llodra  
First Selectman

  
\_\_\_\_\_  
Robert G. Tait  
Finance Director

**PURCHASING AUTHORITY**



## TOWN OF NEWTOWN PURCHASING AUTHORITY

### TOWN OF NEWTOWN, CONNECTICUT INSTRUCTIONS TO PROPOSERS

1. Submit proposals in a sealed envelope plainly marked to identify the particular proposal.
2. The Purchasing Authority of the Town of Newtown reserves the right to accept or reject any or all options, bids, or proposals; to waive any technicality in any bid, or part thereof, and to accept any bid deemed to be in the best interest of the Town of Newtown, Connecticut.
3. Proposers may be present at the opening of the proposals.
4. Proposals may be held by the Town of Newtown for a period not to exceed thirty (60) days from the opening of the proposals for the purpose of reviewing the proposals and investigating the qualifications of the proposers prior to award.
5. It is the sole responsibility of the proposer to see that the proposal is in the hands of the proper authority prior to the proposal opening time.
6. The successful proposer may be required to post a Certificate of Insurance, with the Town of Newtown named as additional insured, in an amount to be determined by the Town of Newtown.
7. Bid Security when required must be by a **certified check, letter of credit or surety bond** for five percent (5%) of the total bid, payable to the Town of Newtown. Surety companies and banks must be satisfactory to the Town of Newtown.
8. Prior to awarding any contract exceeding \$25,000.00 for the construction, alterations or repair for any public building or public work, a labor and materialmen's bond must be furnished by the person to whom the contract is awarded.
9. Performance Bond when required must be by a **certified check, letter of credit or performance bond** for one hundred percent (100%) of the total bid. Surety companies and Banks must be satisfactory to the Town of Newtown.

NEWTOWN MUNICIPAL CENTER  
3 PRIMROSE STREET  
NEWTOWN, CONNECTICUT 06470  
TEL. (203) 270-4221 / FAX (203) 270-4205



E. PATRICIA LLODRA  
FIRST SELECTMAN  
ROBERT G. TAIT  
FINANCE DIRECTOR

[www.newtown-ct.gov](http://www.newtown-ct.gov)

## **TOWN OF NEWTOWN**

### **PURCHASING AUTHORITY**

#### **DIRECTIONS:**

To the new Municipal Center, Town of Newtown, 3 Primrose Street:

From I-84 East and West: Take Exit 11. At end of ramp take a left at light onto Wasserman Way. At next light take a left onto Trade's Lane. At stop sign take a right onto Keating Farms Rd. At next stop sign take a left onto Simpson Street. We are the 2<sup>nd</sup> building on the left.

From RT. 25 Northbound: At intersection of RT. 25 and Mile Hill South take a right at the light. At the next light take a right onto Trade's Lane. At stop sign take a left on to Keating Farms Road. At next stop sign take a left onto Simpson Street. We are the 2<sup>nd</sup> building on the left.

From RT. 25 Southbound: At intersection of RT. 25 and Mile Hill South take a left at the light onto Mile Hill South. At the next light take a right onto Trade's Lane. At stop sign take a left on to Keating Farms Road. At next stop sign take a left onto Simpson Street. We are the 2<sup>nd</sup> building on the left.

**NOTE:** If you are using a GPS, use 28 Trades Lane as the reference road. This will bring you into the Fairfield Hills Complex.

# Summary of Connecticut's Prevailing Wage Law

Last Updated:

**You are here:** [DOL Web Site](#) › [Wage and Workplace Issues](#) › Summary of Connecticut's Prevailing Wage Law

Connecticut's prevailing wage law is codified in Connecticut General Statutes Section Section 31-53 and 31-53a. The law applies to each contract for the construction, remodeling, refinishing, refurbishing, rehabilitation, alteration or repair of any public works project by the State or its agents, or by any political subdivision of the State.

**Coverage:** Conn. Gen. Stat. Section 31-53(g) provides monetary thresholds which must be met before the law is applicable. The prevailing wage law does not apply where the **total cost of all work to be performed by all contractors and subcontractors** in connection with new construction of a public works project is less than four hundred thousand (\$400,000) dollars. The prevailing wages law does not apply in connection with remodeling, refinishing, refurbishing, rehabilitation, alteration or repair of any public works project under one hundred thousand (\$100,000) dollars.

**Prevailing Rate:** The prevailing rate consists of a base rate and a fringe benefit rate which may be paid in cash or benefits. Conn. Gen. Stat. Section 31-53(d) permits the Labor Commissioner to adopt and use the prevailing wage rate determinations as have been made by the Secretary of Labor of the United States under the provisions of the Davis-Bacon Act, as amended. **The agent empowered to let such contract shall contact the Labor Commissioner at least ten, but not more than twenty days, prior to the date such contracts will be advertised for bid, to ascertain the proper prevailing rate. Under Connecticut General Statutes, 31-55a the rates will be adjusted annually on or before July 1st of each year. These new rates will be on the Department of Labor website.**

**Certifications:** Both the Contractor and the Contracting Agent must provide certifications to the Labor Commissioner. Prior to the award of any contract subject to the prevailing wage law, the contracting agent shall certify in writing to the Labor Commissioner the total dollar amount of work to be done in connection with the public works project, regardless of whether such project consists of one or more contracts. Upon the award of a contract subject to the prevailing wage law, the contractor who is awarded the contract shall also certify, under oath, to the Labor Commissioner the pay scale to be used by the contractor and any of his subcontractors for the work to be performed under the contract. Additionally, each employer subject to the prevailing wage law must file certified payrolls with the contracting agent including information,

including but not limited to, employee names; occupations; hours worked; rates paid; and the employers compliance with various provisions of law.

**Penalties:** There are various civil, criminal and administrative penalties for violations of the prevailing wage law. Failure to pay the prevailing rate is a crime which may be a felony depending upon the amount of unpaid wages. Knowingly filing a false certified payroll or failure to file a certified payroll is a Class D felony for which an employer may be fined up to five thousand dollars, imprisoned for up to five years, or both. Disregarding obligations under Conn. Gen. Stat. Section 31-53 may result in an administrative debarment which may preclude any firm, corporation, partnership or association in which such person or firms have an interest from receiving an award of a contract until a period of up to three years have elapsed. Additionally, civil penalties of \$300 per violation of law may also be assessed upon the employer.

Effective October 1, 2005, Public Act 05-50: Any person performing the work of any mechanic, laborer, or worker shall be paid prevailing wages ~~

All persons who perform work on site must be paid prevailing wage for the appropriate mechanic, laborer, or worker classification;

All certified payrolls must list the hours worked and wages paid to all persons who perform work on site regardless of their ownership, i.e.: (Owners, corporate officers, LLC members, independent contractors, et. al);

Reporting and payment of wages is required regardless of any contractual relationship alleged to exist between the contractor and such person.

**For additional information contact:  
Wage and Workplace Standards Division  
Public Contract Compliance  
(860)263-6542**

# **Sandy Hook Streetscape – Phase II – Project Description**

## **Request for Proposal/Bids for the Design & Installation of Defined Streetscape**

### **May 2012**

#### **Introduction**

Proposals are sought for the design and construction of certain streetscape improvements in Sandy Hook center as outlined herein. The selected consultant/installer is expected to prepare design plans for the installation of the specific streetscape elements pursuant to the Sandy Hook Streetscape Specifications dated March 2010 which are available in digital format from the Economic and Community Development Office. The Sandy Hook Streetscape Phase 2 concept plans dated 1/21/2005 may be utilized for reference/informational purposes. A certain map showing the limits of the project is also available. Following approval of the design phase, the contractor will then install the streetscape in accordance with budget allowances.

#### **Limits of Project**

##### ***1. Intersection***

The goal is to upgrade the intersection which will serve to improve the safety of vehicular and pedestrian traffic flow while at the same time serving to upgrade the visual enjoyment of the intersection and the commercial district. The plan is to add new pedestrian amenities for pedestrian crossing safety including an upgraded traffic signal that will have a dedicated pedestrian phase and pedestrian signage; provide for crosswalks at each of the four roads with ADA approved curbing/sidewalks; add streetscape elements that will define pedestrian areas and sidewalks;; remove the island from the intersection, relocate the traffic signal and utility lines from the pole, and better align the crossing roadways, Glen Road and Washington Ave for vehicular travel.

The traffic signal will be moved and upgraded by CT DOT. A dedicated pedestrian phase and a delayed green for Glen Road to permit left hand turns from Washington Avenue onto Church Hill Road are planned. Pedestrian crossing buttons will be installed at the four corners.

All utility wires will be moved by the respective utility companies to allow the removal of the island from the intersection. Newtown Dept. of Public Works is coordinating the project with CT DOT and the utility companies.

The contractor will be responsible for designing and installing the streetscape improvements for the intersection in conjunction with the signal and utility work to be completed by others. The limits of the work subject to this RFP is described herein and are budget dependent.

Newtown Forest Association owns and maintains the plaza on the southwest side of the intersection. A small area within the plaza has failed and is to be repaired. A grass strip adjacent to the bridge and the existing poured concrete (along Church Hill Road) walk way is to be replaced with the poured stamped concrete to widen the sidewalk here. Curbs are to be lowered and ADA approaches for the crosswalks

**Sandy Hook Streetscape – Phase II – Project Description**  
Request for Proposal/Bids for the Design & Installation of Defined Streetscape  
May 2012

are to be installed. Landscaping within the plaza area will be subject to review by the owner and is likely to be a shared responsibility.

The privately owned property at 1 Riverside Road will be greatly impacted by the roadway and utility work. New property lines/ROW mapping is completed. The design and installation of the streetscape improvements for this property will be necessary. A retaining wall and a protective but decorative fence along the top of the wall can be expected. More information will be available on the type of wall to be installed will be available at the pre-bid meeting.

2. ***Glen Road*** (state highway 816)

The design will focus on extending the streetscape from the intersection property (1 Riverside Road) along the east side of Glen Road to 10 Glen Road (Raymond Ruzek – M40-B4-L16ltA). Installation will be subject to the approvals of the property owners and budget constraints. Existing shrubbery, grades, landscaping, utility poles and buildings will pose some constraints for the work. Design should allow for the future continuance of the streetscape elements northward toward the Dayton Street Bridge.

Future plans call for extending the streetscape along the west side of Glen Road as well to extend it from the intersection to the Dayton Street Bridge.

3. ***Riverside Road***

The design will focus on extending the streetscape improvements from the intersection at 1 Riverside Road to a point across the existing driveways for this property. The intersection design will be based on the new ROW plans on file with DPW. The design should allow for the future continuance of the streetscape along the north side of Riverside Rd. to the cemetery.

Design for the improvements along the south side of the roadway from the intersection (2 Riverside Road) to 4-6 Riverside Road (Vasam Assoc. LLC) have been completed and are on file in the land use office. The completed plans may be utilized for reference. The future plan is to extend the streetscape along both the north and south sides of Riverside Road. While the north side is expected to end at the cemetery, the south side will extend to Dickinson Drive.

4. ***Washington Avenue***

The design will focus on taking the streetscape from the intersection on the east side of Washington Avenue to Crestwood Street. Plans for the improvements to 2 Riverside Road are on file in the land use office and should be utilized for reference for this property. The streetscape design for this property at the intersection will likely need revisions due to changes to the ROW and the island removal plan.

The area in front of the building at 4 Washington Street should be reviewed. It currently steps up from the plaza and then steps down to the parking lot serving the same. It is desirable to level out the

**Sandy Hook Streetscape – Phase II – Project Description**  
Request for Proposal/Bids for the Design & Installation of Defined Streetscape  
May 2012

sidewalk and remove the steps in this area. One way would be to lower the grade of the walk in front of the building. The necessary elements to improve the pedestrian safety and access along the front of the property and extend the walk across the driveway should be identified.

**5. Church Hill Road**

Work on Church Hill Road will be limited to the demolition and installation of necessary elements (ADA surfaces/curbing) for the crosswalk at the intersection, the removal of grass between the bridge and existing walkway on the south side of the bridge and the installation of pavement in lieu thereof.

**Project Responsibilities and Oversight**

Field oversight of the installation of the streetscape elements and R.O.W. elements will be by Newtown Dept. of Public Works (DPW) and the Town Engineer. Design and budget will be overseen by the Director of Economic and Community Development (ECD). Property and business owner relations will be the responsibility of DPW and ECD. Ongoing communications and business/owner relations will be coordinated by town staff and will include communications with the Sandy Hook Organization for Prosperity (SHOP) and its streetscape oversight committee. Selection of the contractor/installer will be the responsibility of the Public Works Director, Town Engineer and Director of Economic and Community Development subject to approval by the Town Purchasing Authority.

It is recognized that work will be accomplished within both local and state highway right of ways (ROW) on CT Rt. 816 (Church Hill & Glen Roads) and local roadways Washington Avenue and Riverside Road. Required permits (electrical/roadways etc.) will be the responsibility of the contractor.

The contractor should expect to communicate with property owners, town staff and SHOP representatives throughout the course of the project. Construction field changes are to be avoided to the greatest extent possible making it necessary to plan for field discussions during the streetscape design phase. All design elements must be completed in advance of construction.

**RFP Deliverables**

Consultant shall submit a lump sum cost for the preparation of the design plans breaking it into the five main areas (intersection, Glen Road, Riverside Road, Washington Avenue and Church Hill Road) of work. The design shall incorporate all streetscape elements including but not limited to grading, curbing, sediment and soil erosion controls, sidewalks, landscaping, driveways, ADA crosswalk surfaces and painted crosswalks, lighting & electrical conduits, retaining walls, demolition of existing sidewalks and

**Sandy Hook Streetscape – Phase II – Project Description**  
Request for Proposal/Bids for the Design & Installation of Defined Streetscape  
May 2012

curbing, removal of shrubbery or other elements as deemed necessary by the consultant and/or together with the staff and possibly with consultation of the oversight committee and property owners.

A unit price for the installation of actual streetscape elements for each of the five areas shall also be submitted. A cost sheet is provided for use in the preparation of this requirement.

Any required permits are the responsibility of the contractor and should be identified in the proposal.

Consultants are to identify the main contact and any sub-contractors/consultants that will be utilized for the project. Primary personnel that will work on the project are to be identified along with experience, licenses etc. References and experience with similar projects should be identified utilizing current GSA Form 330 part I and part II.

Together, the lump sum and unit price will determine the actual limits of the project which are budget dependent.

A CT licensed P.E. may be necessary for certain retaining walls and drainage designs. A CT licensed electrician will be necessary for the lighting work.

A time frame for the start and completion of the project shall be included.

**A mandatory pre-bid meeting will be conducted starting at 9 am on Thursday, May 24<sup>th</sup> in the Council Chambers Meeting Room at the Newtown Municipal Center, 3 Primrose Street, Newtown, CT.**

**Four (4) copies of the proposals, together with required information outlined above should be addressed to: Robert G. Tait, Finance Director and must be received by 2:00 P.M., June 15, 2012 in the Newtown Finance Dept., 3 Primrose Street, Newtown, CT 06470.**

**Project Budget**

The intersection streetscape work will be paid for with Town and State funding. Additional streetscape funding may become available by combining town and private funding sources. Funds in the amount of \$500,000 are currently available and dedicated for the immediate project. Future budget allocations may be expected.

**Newtown is an equal opportunity/affirmation action employer  
Minority and Women Owned Enterprises are encouraged to apply**

List of Lighting Fixture Distributors  
for the Sandy Hook Streetscape Project  
June 16, 2005

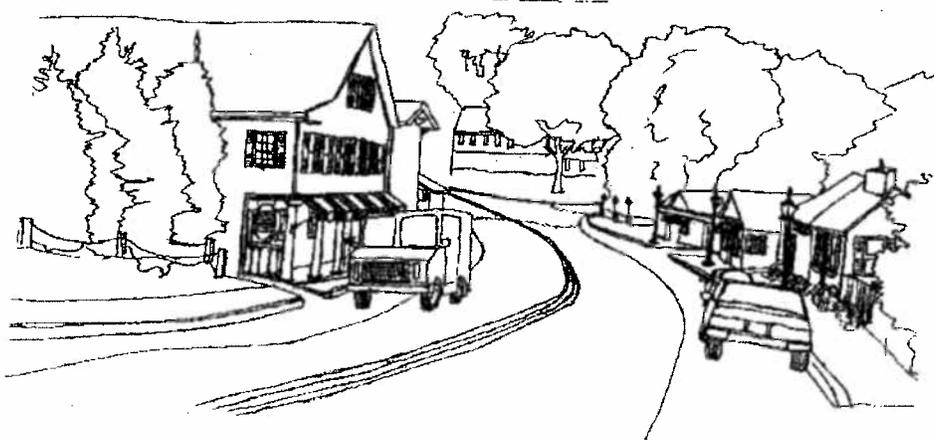
Vanguard Lighting  
333 Pleasant Valley Road  
South Windsor, CT 06074  
ph: 860.282.0597 • fax: 860.282.0857  
Contact: Dean Rosa 203-732-6105 (Cell)

F & M Elec.  
29 Federal Rd.  
Danbury, CT 06810  
ph. 203-744-7445  
fx. 203-744-3640  
Contact:- Fil Cerminara

Electrical Wholesalers  
39 Newtown Rd.  
Danbury, CT 06810  
ph. 203-744-1143  
fx. 203-798-7882  
Contact: Tony Picarski

Diamond Elec.  
12 North Street  
Danbury, CT 06810  
ph. 800-458-6592  
fx. 203-798-1504

# SANDY HOOK DESIGN DISTRICT STREETSCAPE SPECIFICATIONS



**TOWN OF NEWTOWN**

March 2010

**SANDY HOOK DESIGN DISTRICT  
STREETScape SPECIFICATIONS**

February 2007

**ACKNOWLEDGEMENTS**

Design Advisory Board

George Reichert, Chairman  
Michael Manes  
Abbe Hocter

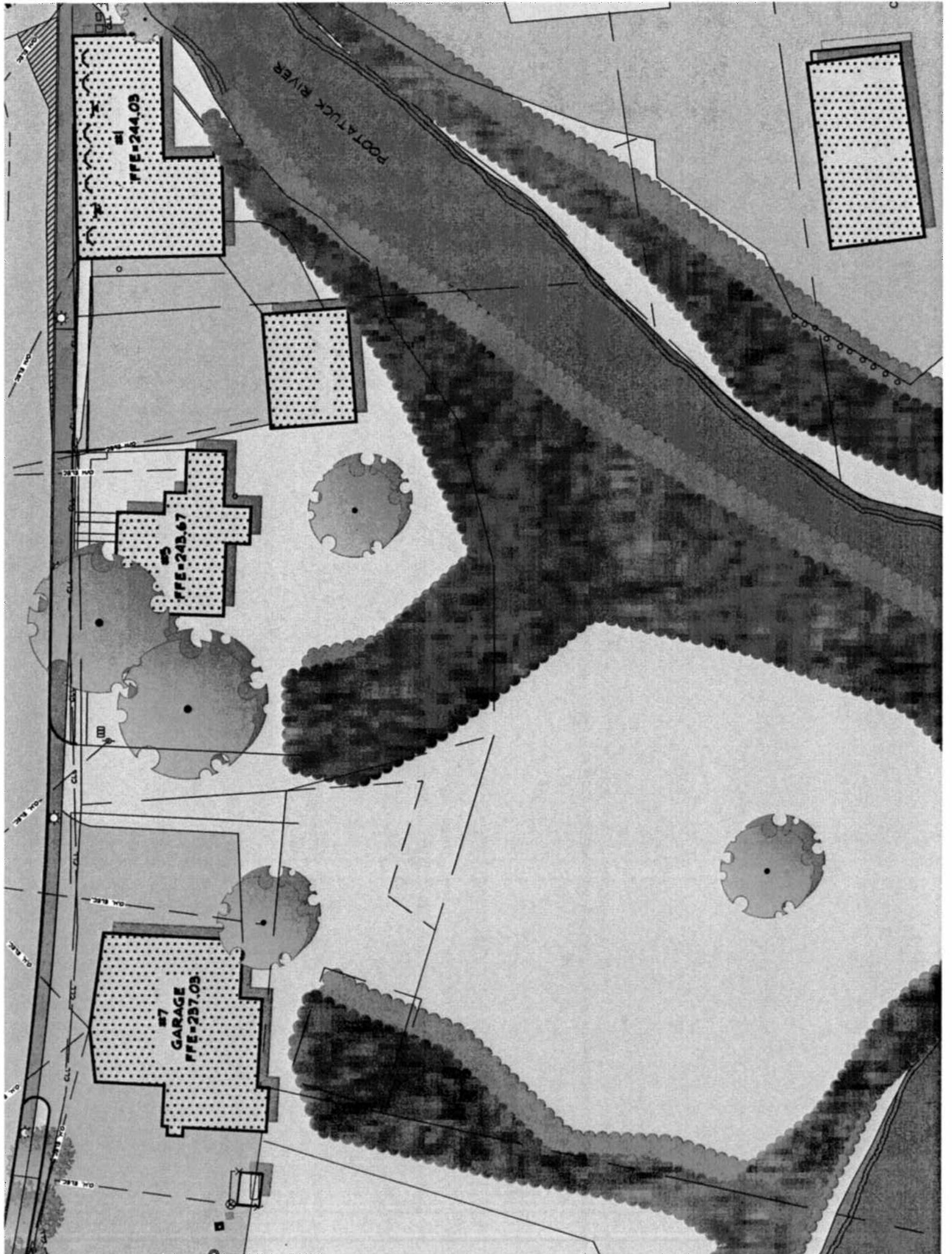
Planning and Zoning Commission

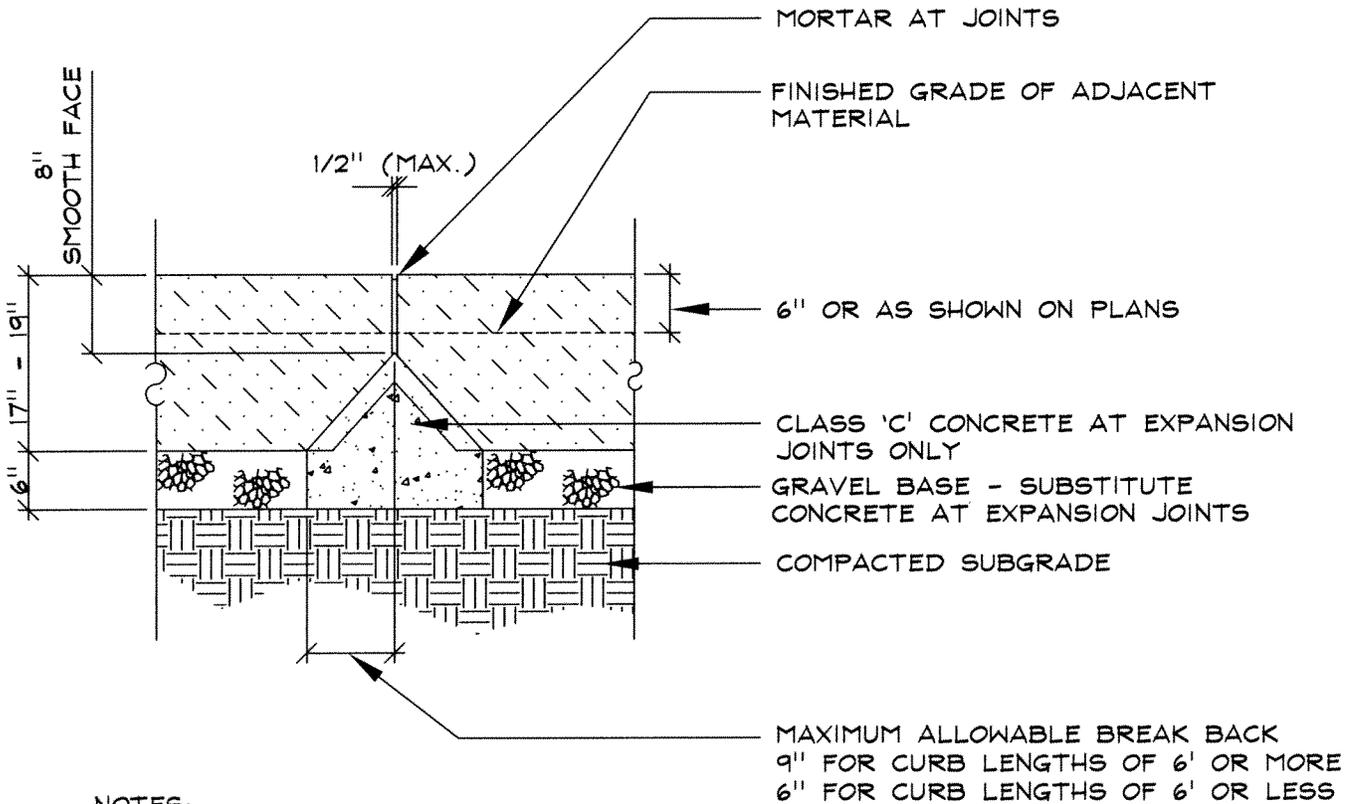
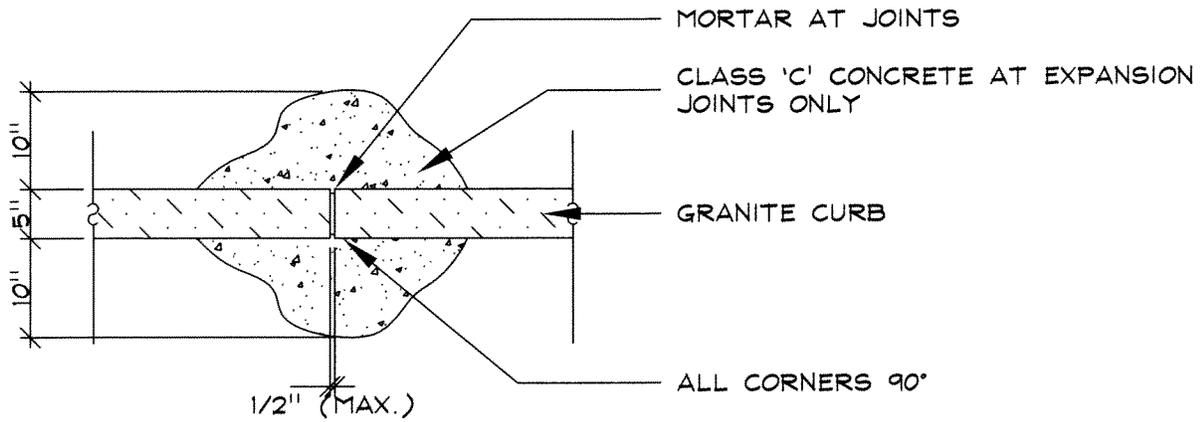
William O'Neil, Chairman  
Lilla Dean, Vice-Chairman  
B. Jane Brymer, Secretary  
Robert Poulin  
Sten Wilson  
Robert Mulholland, Alternate  
Dennis Bloom, Alternate  
Philip Cruz, Alternate

STAFF

Elizabeth Stocker, AICP, Director of Community Development  
Fred Hurley, Director of Public Works  
Ronald Bolmer, Town Engineer

Sketch Courtesy of Annie Lux





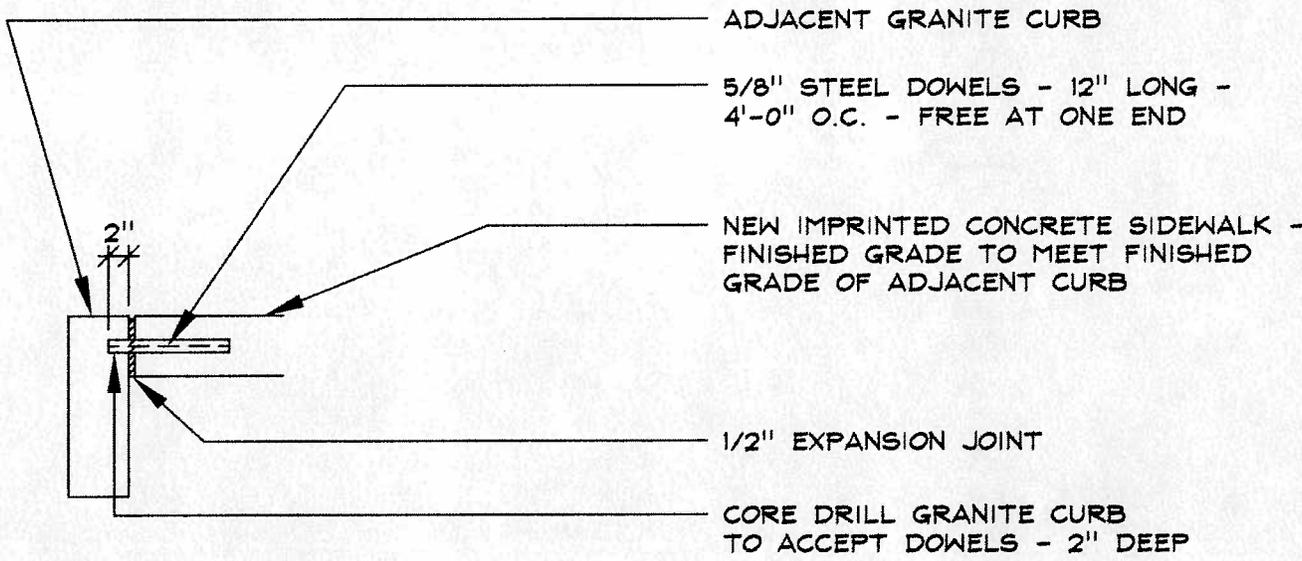
**NOTES:**

- 1) GRANITE CURBING SHALL CONSIST OF APPROVED GRANITE, FURNISHED IN ACCORDANCE WITH THE DIMENSIONS AND DETAILS OF THE PLANS OR AS DIRECTED
- 2) GRANITE CURBING SHALL BE PLACED IN ACCORDANCE TO THE PROVISIONS OF SECTION 8.13 OF THE STATE OF CONNECTICUT, DEPARTMENT OF TRANSPORTATION, STANDARD SPECIFICATIONS FOR ROADS, BRIDGES AND INCIDENTAL CONSTRUCTION, FORM 816 WHERE APPLICABLE
- 3) MATERIALS FOR THIS WORK SHALL CONFORM TO REQUIREMENTS OF ARTICLES M.12.06 FOR GRANITE CURBING, M.11.04 FOR MORTAR, IF REQUIRED, AND M.02.03 FOR GRAVEL BASE



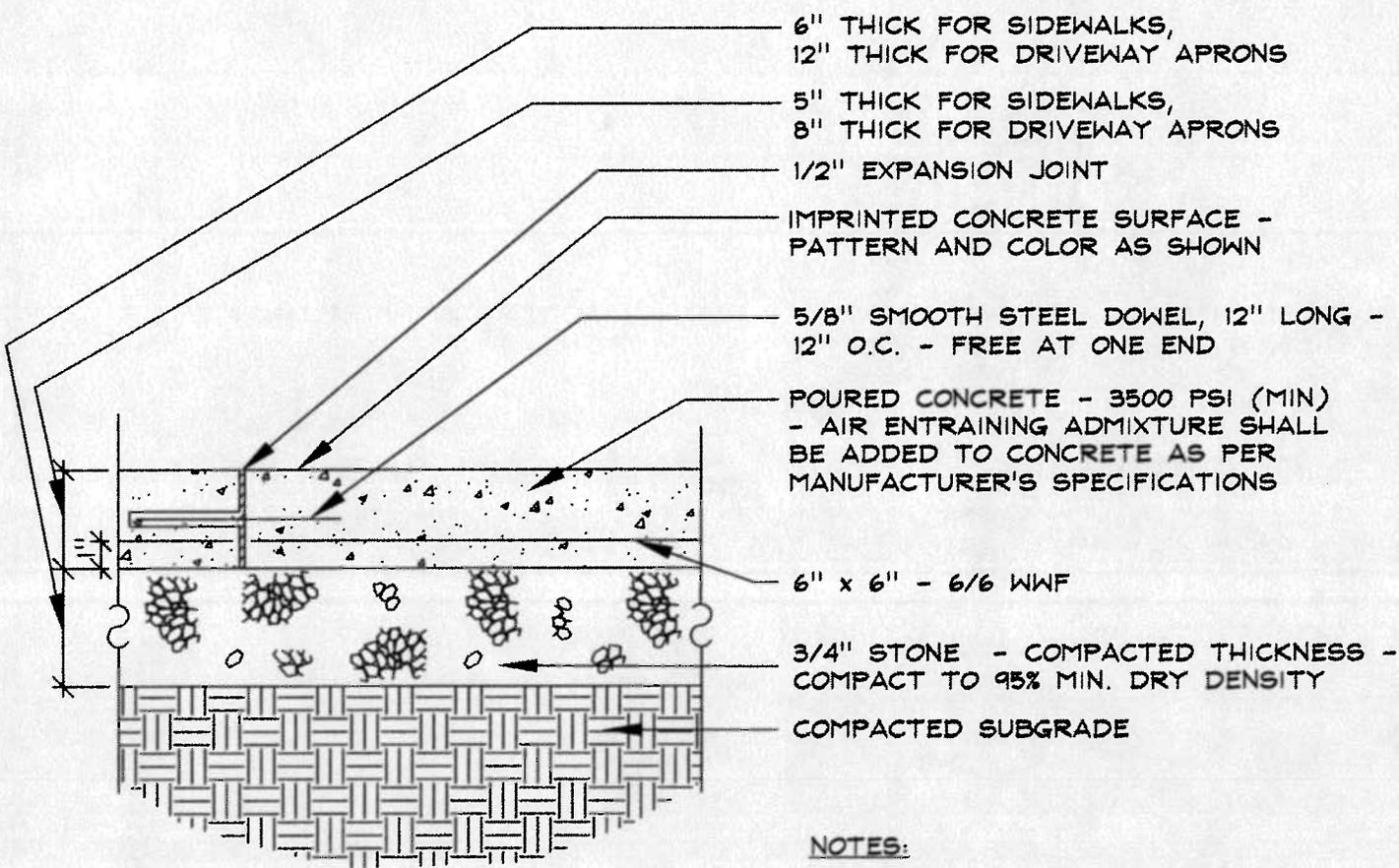
**GRANITE CURB**

NOT TO SCALE



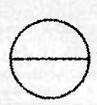
- ADJACENT GRANITE CURB
- 5/8" STEEL DOWELS - 12" LONG - 4'-0" O.C. - FREE AT ONE END
- NEW IMPRINTED CONCRETE SIDEWALK - FINISHED GRADE TO MEET FINISHED GRADE OF ADJACENT CURB
- 1/2" EXPANSION JOINT
- CORE DRILL GRANITE CURB TO ACCEPT DOWELS - 2" DEEP

GRANITE CURB TO SIDEWALK CONNECTION

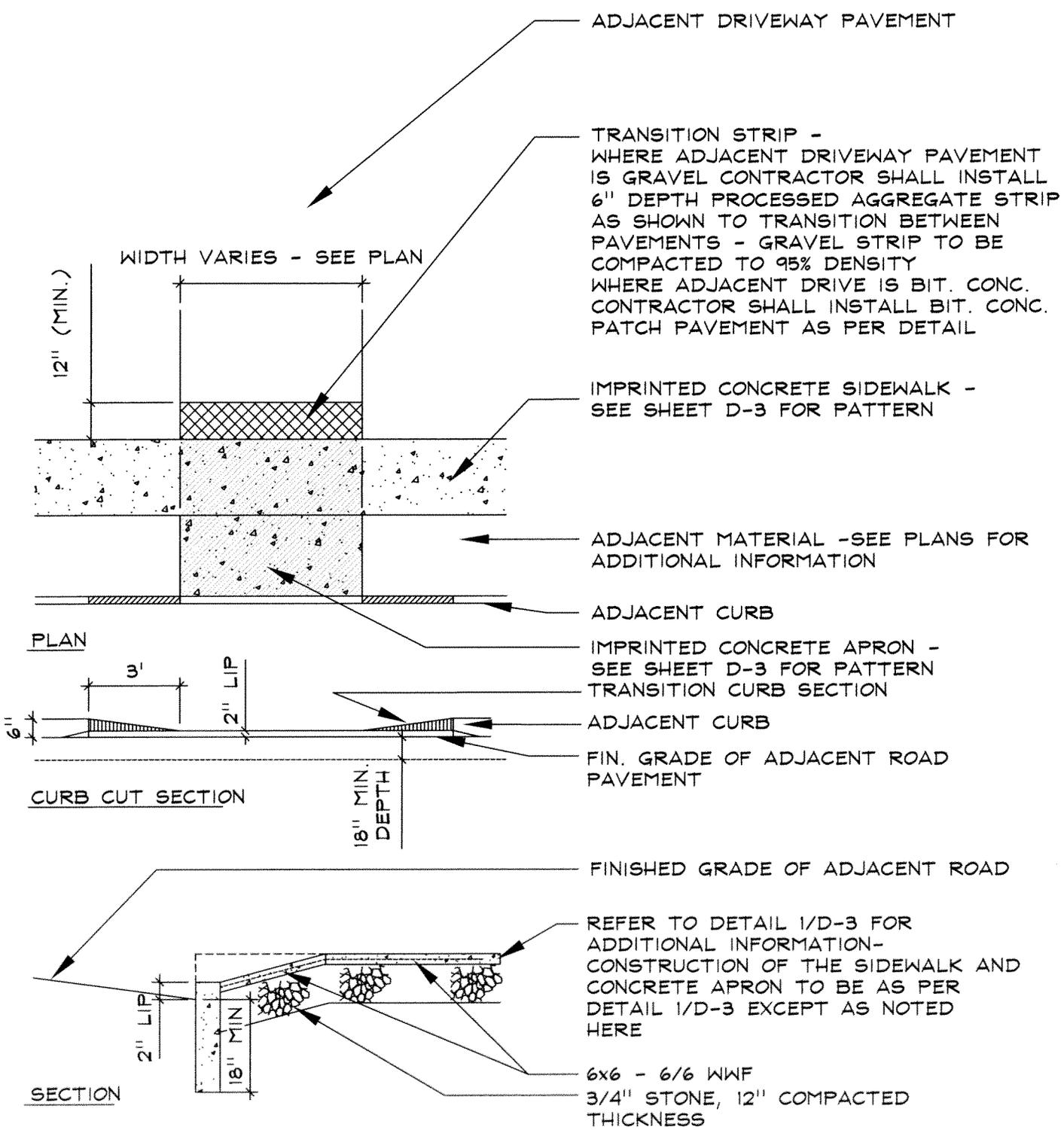


- 6" THICK FOR SIDEWALKS, 12" THICK FOR DRIVEWAY APRONS
- 5" THICK FOR SIDEWALKS, 8" THICK FOR DRIVEWAY APRONS
- 1/2" EXPANSION JOINT
- IMPRINTED CONCRETE SURFACE - PATTERN AND COLOR AS SHOWN
- 5/8" SMOOTH STEEL DOWEL, 12" LONG - 12" O.C. - FREE AT ONE END
- POURED CONCRETE - 3500 PSI (MIN) - AIR ENTRAINING ADMIXTURE SHALL BE ADDED TO CONCRETE AS PER MANUFACTURER'S SPECIFICATIONS
- 6" x 6" - 6/6 WWF
- 3/4" STONE - COMPACTED THICKNESS - COMPACT TO 95% MIN. DRY DENSITY
- COMPACTED SUBGRADE

NOTES:  
 1) SEE SPECIFICATIONS FOR ADDITIONAL INFORMATION



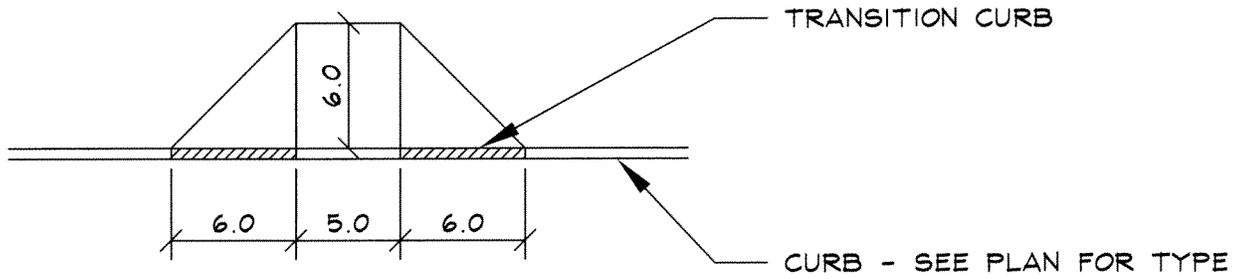
IMPRINTED CONCRETE DRIVEWAY AND WALK PAVEMENT  
 NOT TO SCALE



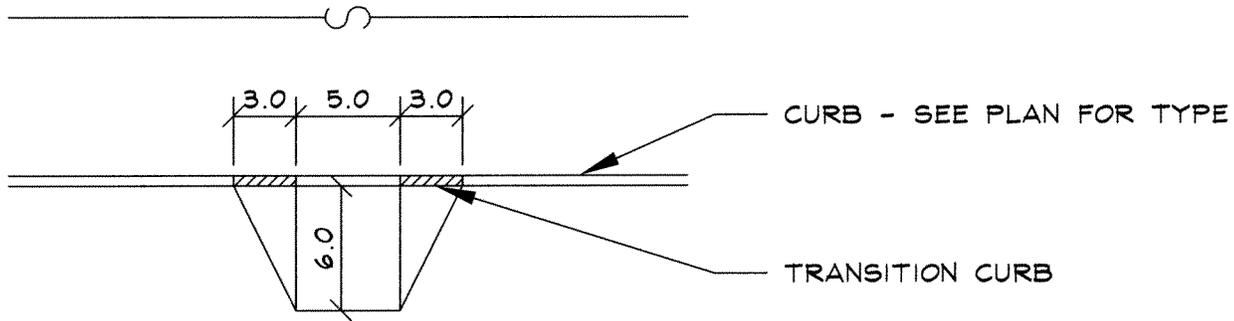
CONC. DRIVEWAY APRON, SIDEWALK & CURB

NOT TO SCALE

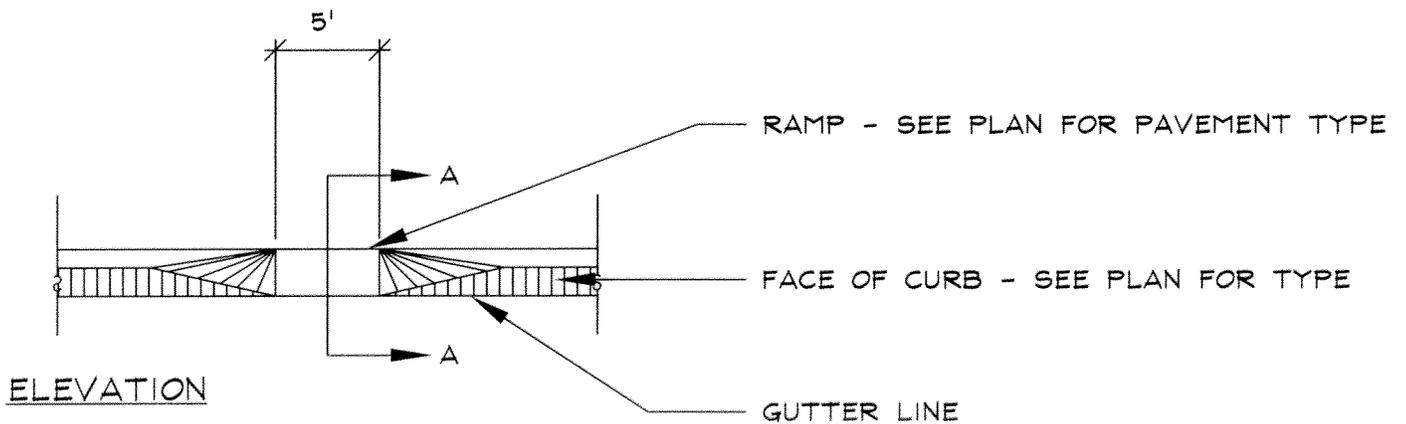
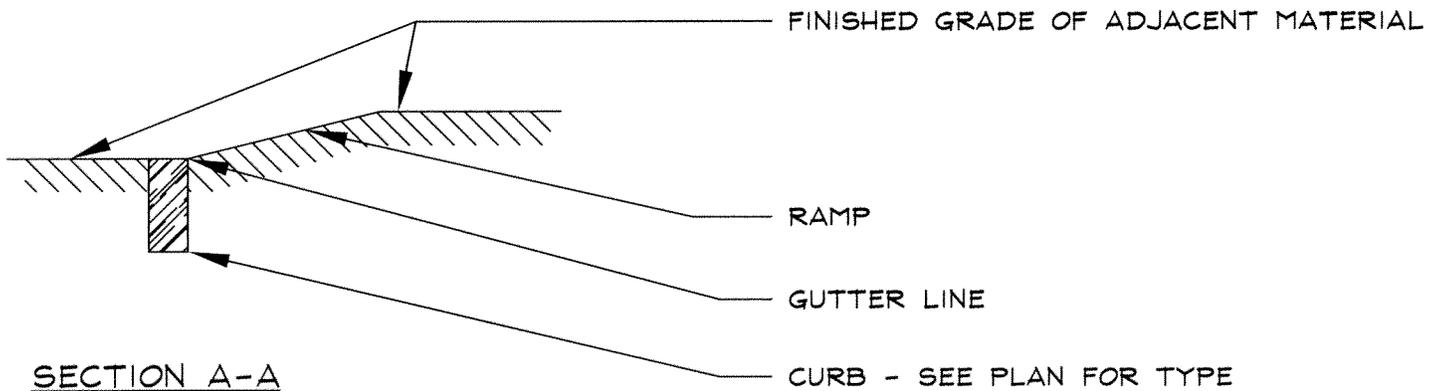




PLAN NORTH SIDE OF CHURCH HILL ROAD

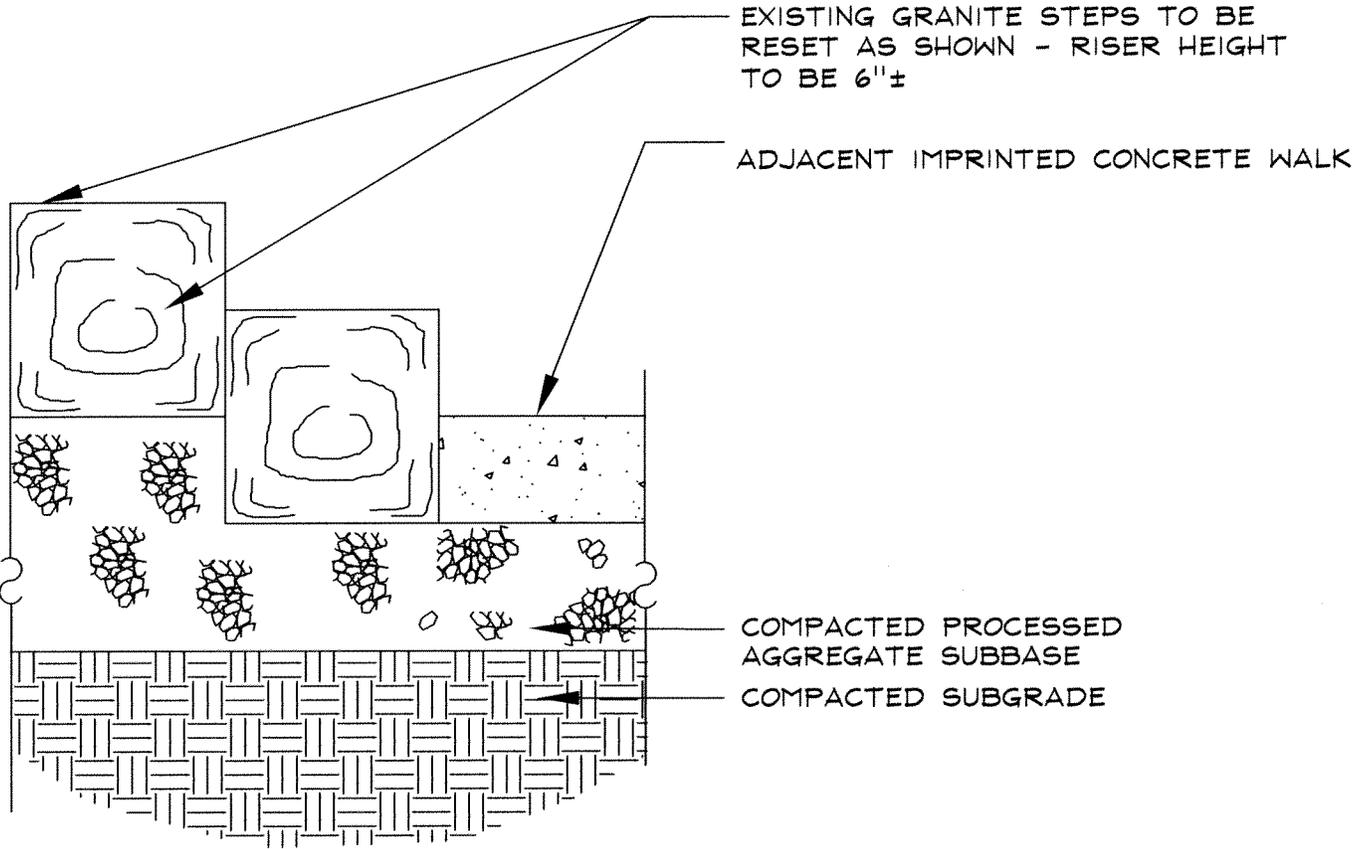


PLAN SOUTH SIDE OF CHURCH HILL ROAD



HANDICAP RAMPS AT MID-BLOCK CROSSWALK

NOT TO SCALE




**GRANITE STEPS**  
 NOT TO SCALE

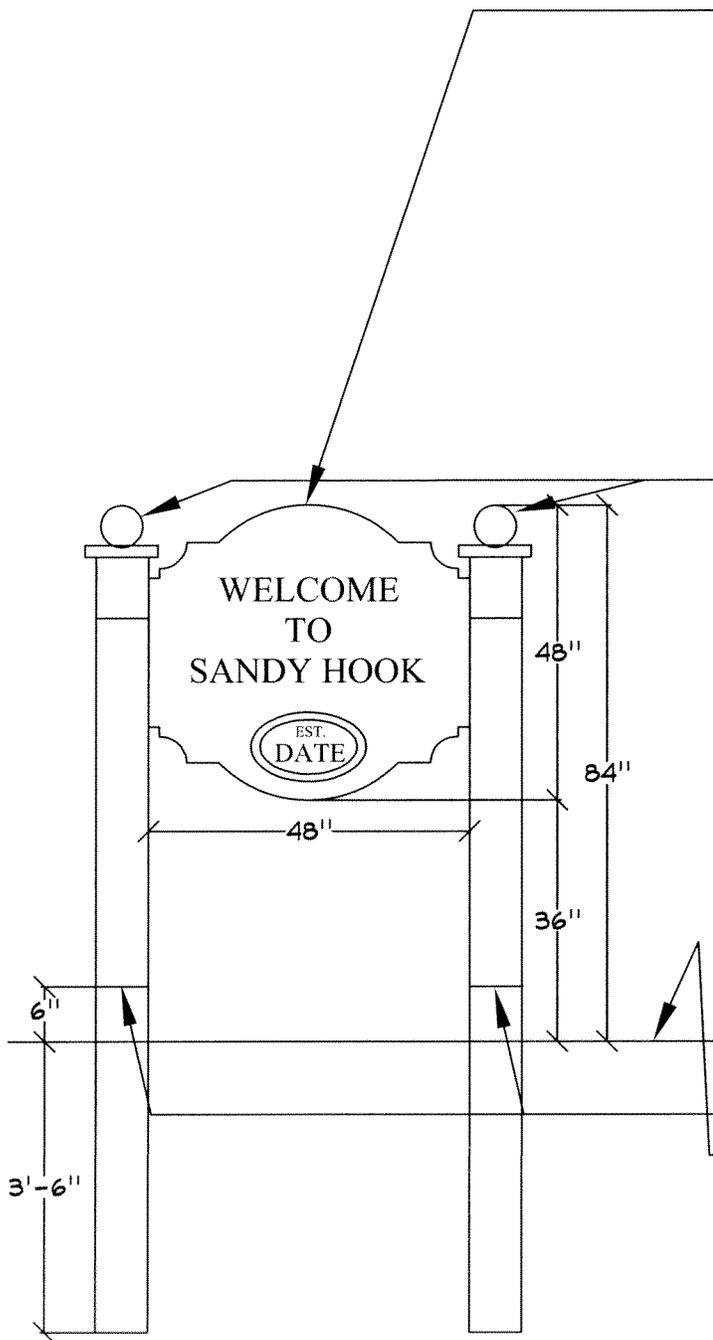
ONE SINGLE FACED, CARVED 48" X 48" GROUND SIGN, CONSTRUCTED OF 3/4" HIGH DENSITY URETHANE LAMINATED TO 3/4", MAXIMUM DENSITY OVERLAY BACKBOARD. LETTERING, LOGO AND EDGE TO BE FINISHED IN 23KT. GOLD LEAF. BACKGROUND TO BE PAINTED BENJAMIN MOORE CHROME GREEN MOORGLO LATEX EXTERIOR PAINT. SIGN AS MANUFACTURED BY GREAT SOUTH BAY DESIGN COMPANY, 587 MIDDLE ROAD, BAYPORT, NY 11705 PHONE: (631)-472-3000 OR APPROVED EQUAL.

POSTS ARE 6" X 6" CCA POSTS PAINTED WITH BENJAMIN MOORE'S BRIGHT WHITE MOOR GLO. POSTS TO BE SET 42" BELOW GRADE(MIN.) IN GRAVEL. FINIALS ARE 5" ROUND AND ARE FINISHED WITH 23 kt GOLD.

NOTE: SUBMIT SHOP DRAWING FOR APPROVAL PRIOR TO MANUFACTURING SIGN

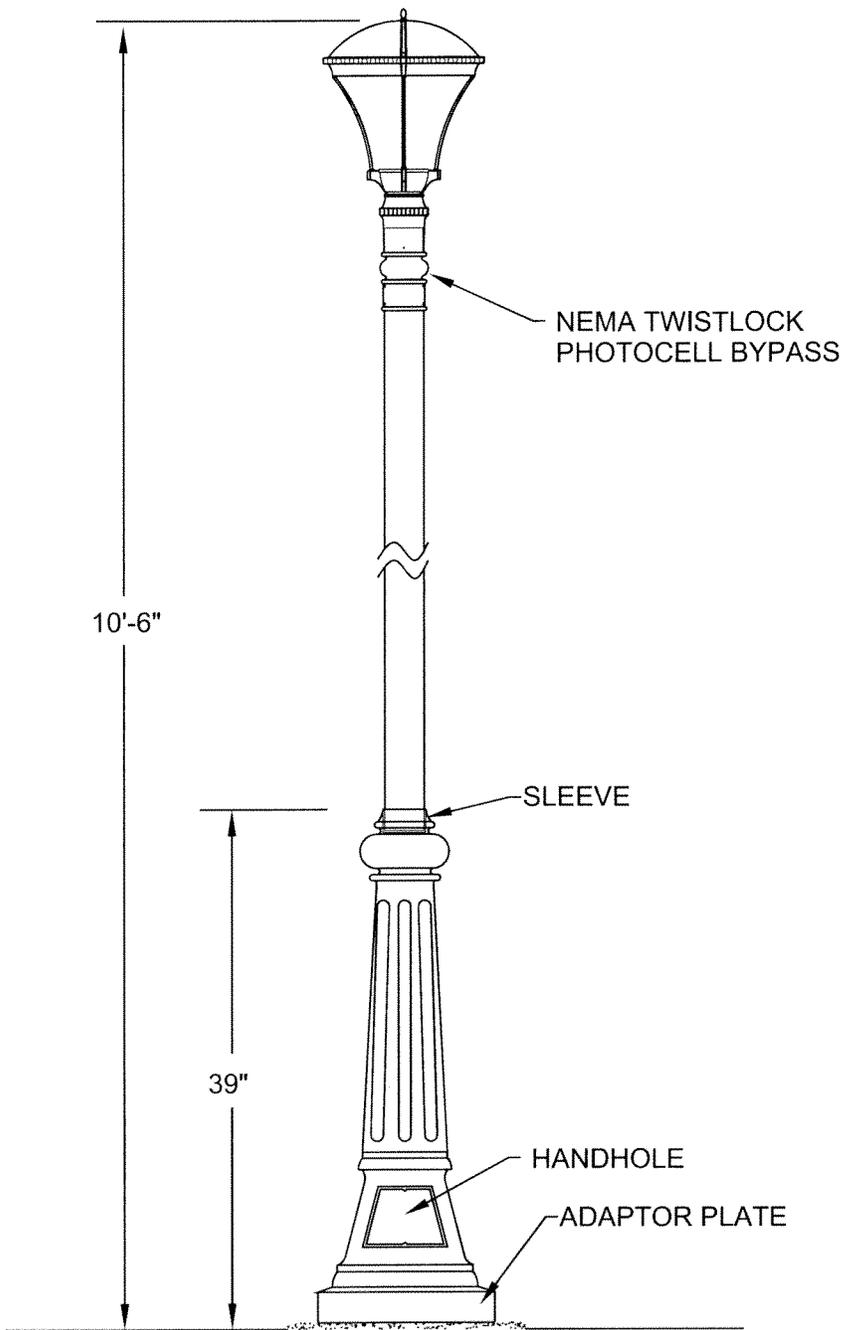
FINISHED GRADE OF ADJACENT MATERIAL

BREAKAWAY POSTS: 2" WOOD DOWEL THROUGH 6" X 6" CCA POST 6" ABOVE FINAL GRADE.



WELCOME SIGN

NOT TO SCALE

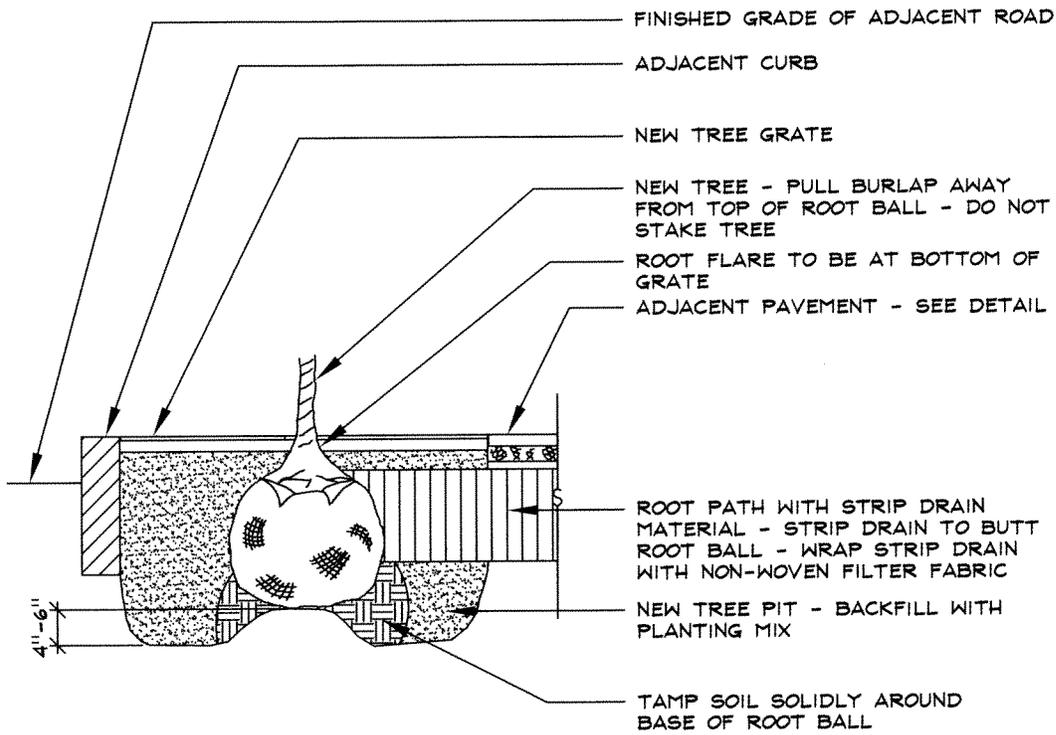


# POLE DETAIL - TYPE A

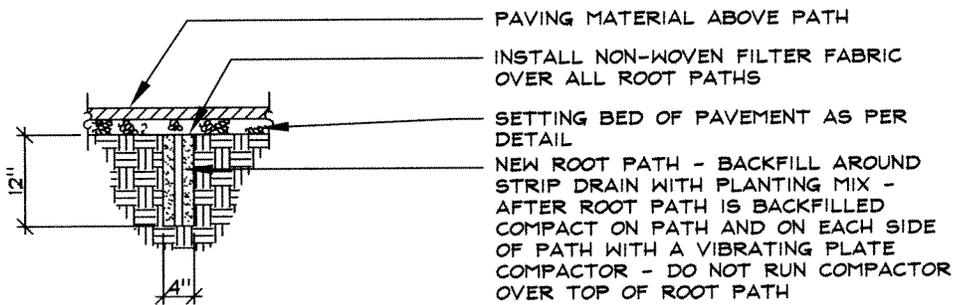
NO SCALE

# SCHEDULE OF LIGHTING FIXTURES

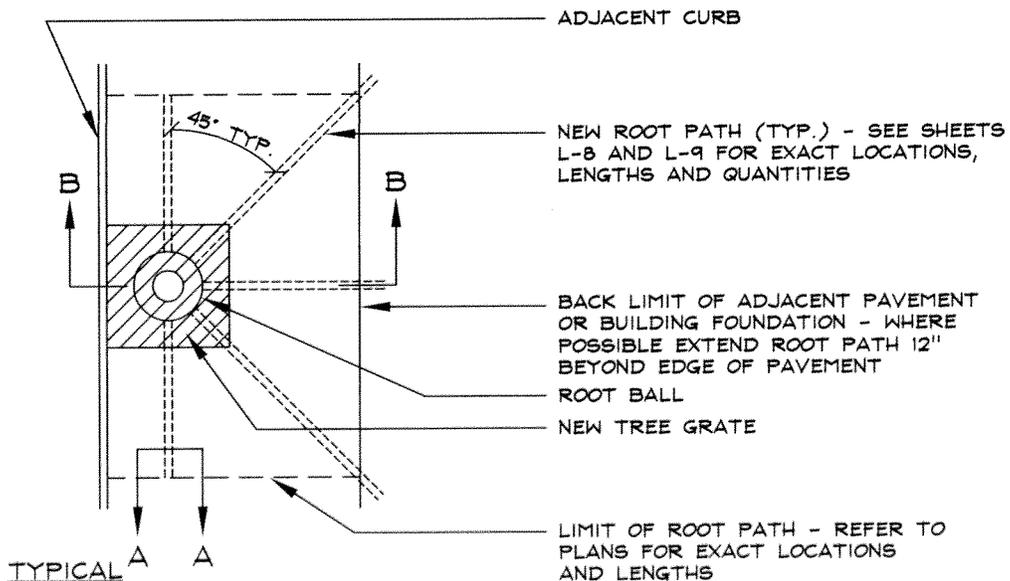
TYPE	LAMPS WATTS	MOUNTING	VOLTAGE	FIXTURE MANUFACTURER AND CATALOG NUMBER	REMARKS
A	1-100W MH	POLE	120V	ARCHITECTUAL AREA LIGHTING - PROVIDENCE	FULL CUTOFF, BLACK ALUMINUM POLE, AND RECEPTACLE AT TOP, DB12 BASE, DIRECT LIGHT FIXTURE, NEMA TWISTLOCK PHOTOCELL BYPASS



SECTION B-B



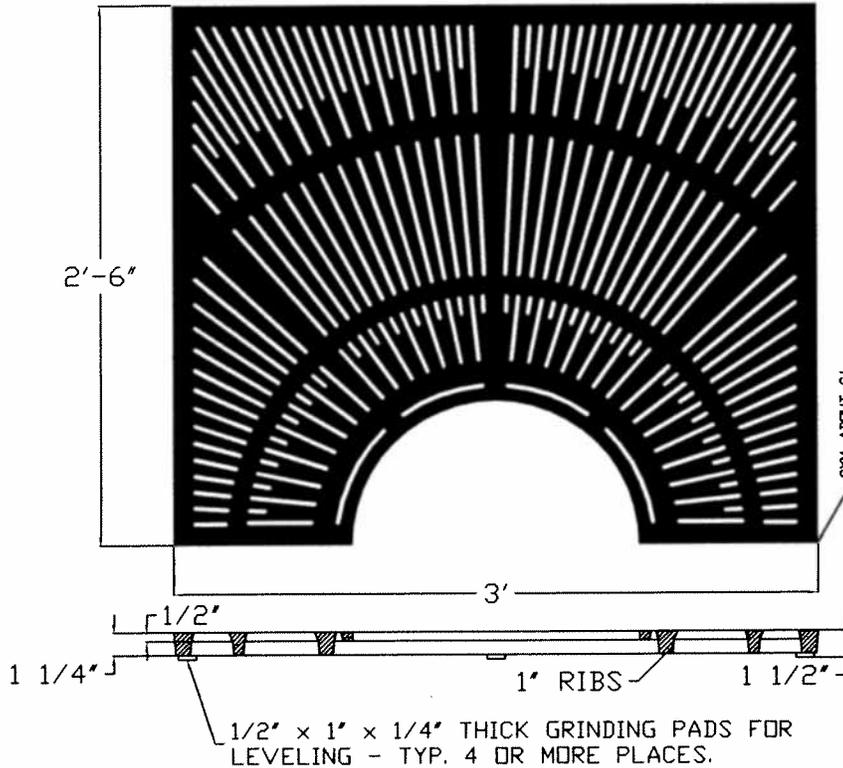
SECTION A-A



# STARBURST

SERIES 1

MODEL SHOWN: M6054-1  
 ©1984 IRONSMITH



GRATE MODEL NO.	SIZE (INCHES)	TREE OPENING	WEIGHT		USE FRAME
			IRON	ALUMINUM	
M3654-1	2' x 3'	16,18,28	166	60	M24 X 36F
M6054-1	3' x 5'	16,18	300	108	M36 x 60F

- Cast with 1/4" maximum slot openings to meet A.D.A. guidelines.
- Tree opening can be expanded to accommodate growth or ordered with the larger opening.
- Available in cast grey iron or cast aluminum these Grates are designed to carry pedestrian loads only
- Aluminum grates should be installed with frame and pilfer proof bolts to prevent unauthorized removal.
- For coating options, please see section on "Finishes".



**IRONSMITH TREE GRATES**  
 FOR LANDSCAPE ARCHITECTURE

(800) 338-4766